



Northumberland

County Council

RECORD OF DECISION TAKEN BY THE DIRECTOR OF ENVIRONMENT AND TRANSPORT

Paul Jones – Director of Environment & Transport

Blyth Town Centre Highway Improvements – The Link (Bridge Street)

Cabinet Member: Councillor John Riddle, Improving our Roads & Highways

04 December 2023

Purpose of Report

This report sets out the background to the proposals to implement new traffic restrictions and traffic calming measures associated with the Energising Blyth highway improvement works in the town centre.

Recommendations

It is recommended that the proposed new traffic calming features and restrictions are implemented in order to accommodate the regeneration works associated with the project.

Link to Corporate Plan

This report is relevant to the following key themes in the Corporate Plan for 2023-26:

- 'Tackling Inequalities' - Creating places where there is equity in access to an environment that encourages physical activity and active travel;
- 'Driving Economic Growth' - Introduce measures which make sustainable travel a more attractive, greener, and easy alternative to getting around including cycling and walking.

Key Issues

1. Highway improvement works are to be carried out in the Bridge Street vicinity in association with the Energising Blyth project, promoting active travel such as walking and cycling in the process.

Delegated Decision

2. Parking and movement restrictions, along with a variety of traffic calming measures, are proposed as part of the works in order to improve road safety and the free flow of traffic as part of this new landscape.
3. The project is endorsed by the local County Councillors for Blyth and Blyth Town Council.
4. The scheme is to be funded through the Energising Blyth programme.

Background

Northumberland County Council has received funding as part of the Energising Blyth project to implement walking and cycling improvements in Blyth town centre. Works to the area include new cycleways and pedestrian crossing facilities, along with proposed traffic calming measures in the form of raised tables and new restrictions involving 20mph speed limits, waiting and loading restrictions, one-way and no entry systems.

Bridge Street is located eastwards of Blyth's town centre and is the route that links with the Quayside. New infrastructure including street lighting, paving, furniture and greenery on Bridge Street aims to provide a welcoming gateway into the town centre similar to recent improvements made as part of the Energising Blyth project.

Proposals

For highway improvement features to the section of Bridge Street between Blyth Market Place and the four-arm roundabout junction with Quay Road to be effective, both static and movement traffic restrictions are required to operate alongside traffic calming features which enhance the area's amenity, whilst providing a safe environment for all road users visiting the area.

Parking Restrictions

The current parking restrictions can often create congestion due to the placement of taxi ranks, bus stops and parking bays and forces motorists to manoeuvre onto both sides of the carriageway when in use by parked vehicles. 'No Waiting at Any Time' restrictions in the form of double yellow lines are proposed throughout the majority of Bridge Street to ensure that the carriageway is kept clear for the free flow of traffic, pedestrians or cyclists have clear visibility when navigating through designated crossing points and that the marked parking bays provided as part of the scheme are made use of.

Double yellow lines are intended to continue beyond the adjoining roads with Bridge Street so that obstructive and indiscriminate parking is prohibited at key junctions which also act as crossing points for pedestrians and cyclists. They are also proposed at the road end points of Stanley Street and the back lane road between Stanley Street and Beaconsfield Street where the footway is to be extended, thereby acting as a turning area and preventing vehicles from encroaching onto the new shared use space.

At Percy Street, the same restrictions are proposed to extend southwards from its junction with Bridge Street to complement a proposed new mandatory cycle lane, which would prohibit vehicles from parking on the kerbside where an

Delegated Decision

unbroken white line and cycle markings are present so cyclists do not encounter vehicles obstructing their route.

Where sections of road have been considered unsafe to permit loading or unloading, or for valid Blue Badge holders to park on, a 'No Loading at Any Time' ban in the form of double kerb blips is proposed on Union Street and Havelock Street at its junction with Bondicar Terrace. In turn, a loading only bay is to be provided on Havelock Street and operational Monday-Saturday between the hours of 8am-6pm so loading or unloading can still be carried out for businesses situated here. Vehicles are also permitted to stop on double yellow lines without kerb blips present for the purposes of loading/unloading.

'No stopping except taxis' restrictions denoted via signage as a taxi rank are proposed in a new formalised lay-by on Bridge Street which improves the free flow of traffic, as opposed to the current arrangements outside Frameworks where the ranks lie directly on the carriageway. The taxi rank will also be located further westwards into the town centre, where footfall is likely to be considerably higher once works to the Market Place are complete.

Limited waiting parking places are proposed in lay-bys again to maintain the free flow of traffic, with the restrictions in the form of:

- A disabled bay on Bridge Street for Blue Badge holders only, applying all days from 9am – 6pm for a maximum stay of 3 hours and no return within 4 hours;
- Limited waiting parking bays on Bridge Street and Percy Street, applying all days from 9am – 6pm for a maximum stay of 30 minutes and no return within 1 hour.

It is intended to use these timings to mirror the existing parking facilities recently implemented on Bowes Street and remain consistent through the town centre, with the aim of improving vehicular turnover and footfall for local businesses in the process. Long-stay parking can still be utilised in the town centre's off-street car parks when required.

An overview of the proposed parking restrictions is presented in Figure 1 below.

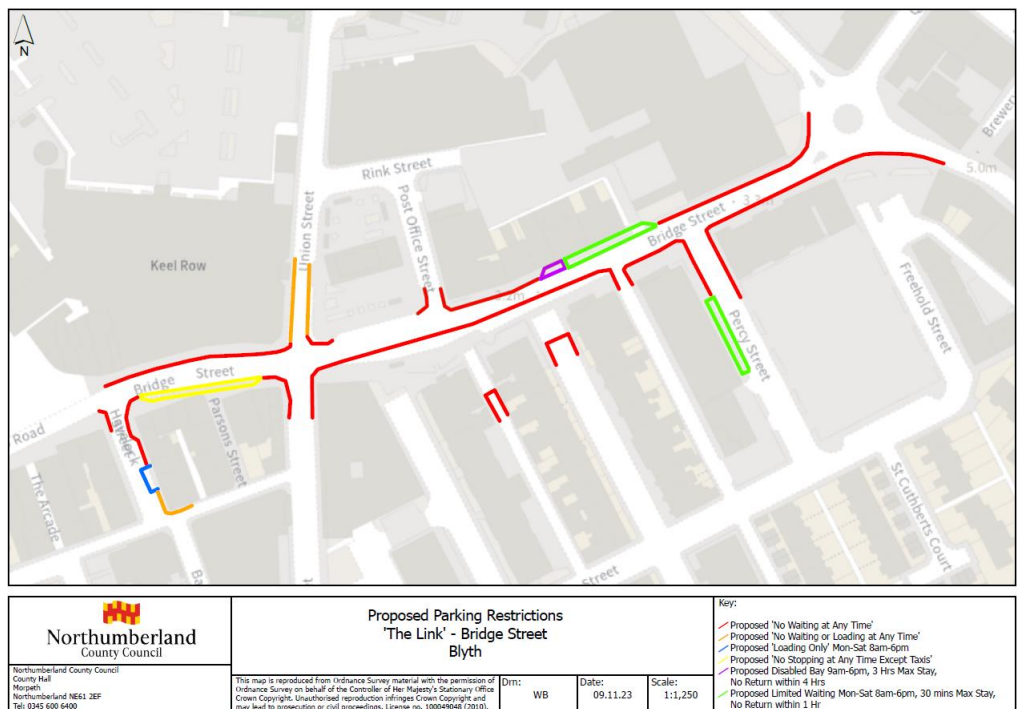


Fig. 1: Proposed new parking restrictions at Bridge Street and its adjoining roads

20 MPH Speed Limit

A reduction in the speed limit to promote slower speeds by motorists is to be implemented across this section through a 20 MPH zone extending from the newly implemented 20 MPH routes in the town centre as part of the project, which will be indicated by roundels in the form of road surfacing and signage, the extents of which can be viewed in Figure 2.

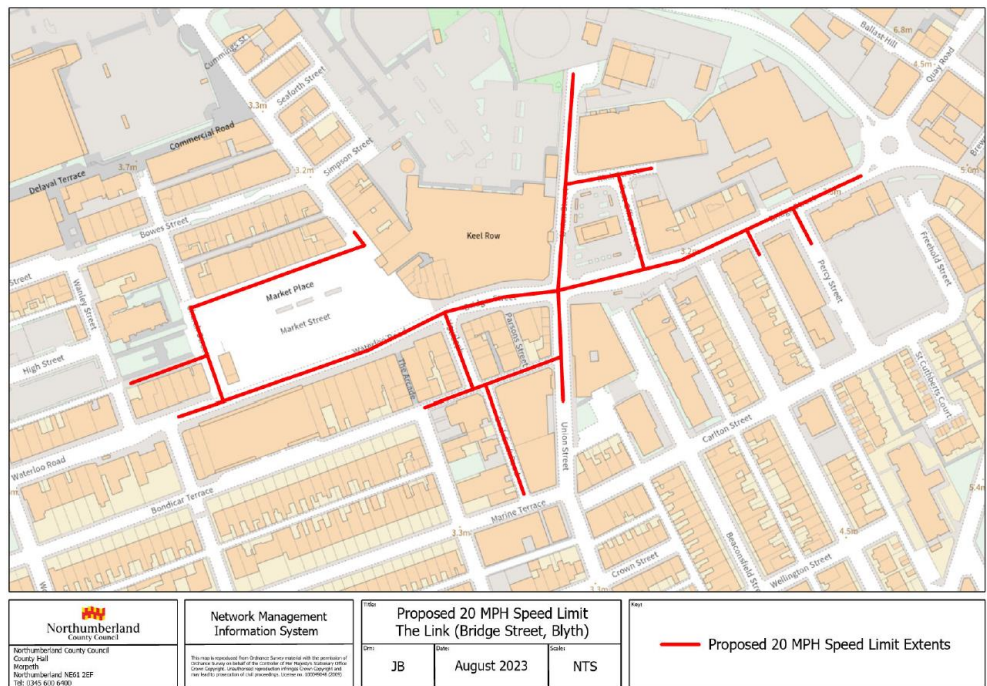


Fig. 2: Proposed 20 MPH area at Bridge Street and its adjoining roads

One-Way, No Entry and Turning Restrictions

Delegated Decision

Further movement restrictions are also proposed as part of the works to improve road safety for all road users and those existing restrictions denoted by signage currently in place are to be formalised through the creation of a Traffic Regulation Order. One-way systems are proposed through Post Office Street (in a northern then southern direction towards the junction of Union Street and Bridge Street), through the back lane between Stanley Street and Percy Street (in a southern direction) and through Havelock Street (in a southern direction) however cyclists are exempt from the latter movement as part of the contra-flow cycle lane to be extended from Bondicar Terrace.

'Prohibition of Entry' points will also be in place at Union Street, the back lane between Stanley Street and Percy Street and at Havelock Street (cyclists will again be exempt from the no entry restrictions into Havelock Street as part of the contra-flow route) to prohibit vehicles from entering into the area via the wrong direction. To ensure these measures are protected, left/right turning bans are also proposed as part of the one-way systems to be installed – every restriction will be denoted by existing and new, improved signage so that the traffic flows follow the layout highlighted on the plan in Figure 3.

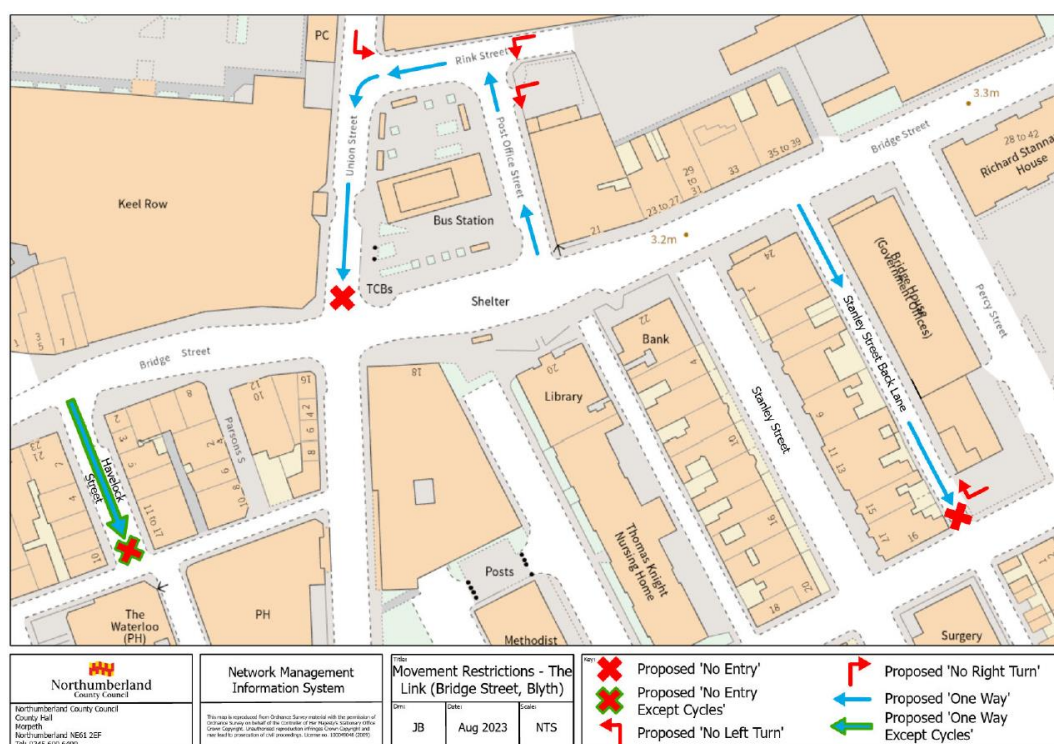


Fig. 3: Proposed movement restrictions at Bridge Street and its adjoining roads

Raised Tables

Road humps in the form of raised tables are proposed as part of the overall works to new pedestrian crossing points which encourage slower speeds on the approach to these facilities and indicate in advance to motorists that pedestrians and cyclists will be present. This feature will cover the pedestrian crossings to be situated at the Bridge Street junctions with Havelock Street, Union Street and further east where a parallel crossing on Bridge Street near the four-arm roundabout is also to be implemented.

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At junction crossing points where it would be beneficial for cyclists to travel on a level surface and to safely pedal through the route continuously, the Bridge Street junction with Percy Street and the junction with the back lane road between Percy Street and Stanley Street will be raised with a marked priority for those cyclists.

The estimated extents of the raised table surfaces throughout the scheme are illustrated in Figure 4 below.

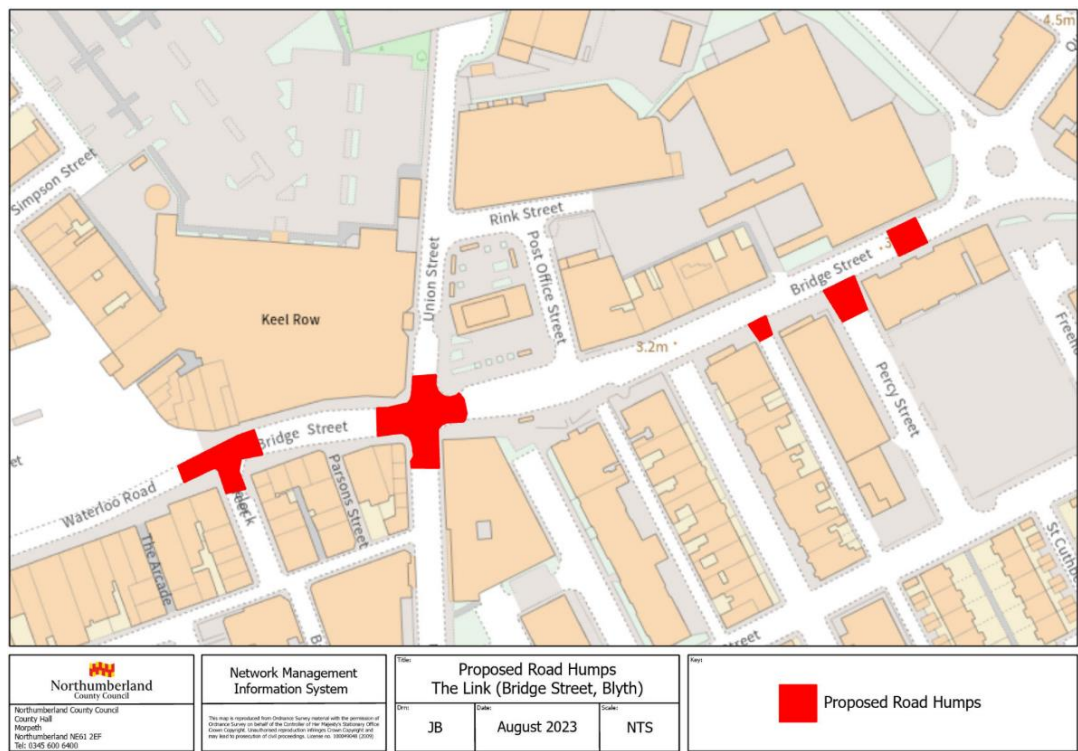


Fig. 4: Proposed road humps at Bridge Street and its adjoining roads

Detailed designs of the above proposals can be found in Appendix A. They are intended to improve road safety, increase active travel, maintain the free flow of traffic where possible and to protect the amenity of the area overall. Northumbria Police, other emergency services, local businesses and relevant statutory consultees will be notified of these amendments as part of the process for making the permanent Orders.

Delegated Decision

Implications Arising out of the Report

Policy	Consistent with existing policies
Finance and value for money	Scheme will be financed by the Energising Blyth programme
Legal	Motorists will be required to comply with the required Traffic Regulation Orders
Procurement	None
Human Resources	None
Property	None
Equalities (Impact Assessment attached) Yes <input type="checkbox"/> No <input type="checkbox"/> N/A <input type="checkbox"/>	N/A
Risk Assessment	The proposals are designed to improve road safety and accessibility for all road users accessing Blyth town centre
Crime & Disorder	Regulations in place to carry out enforcement
Customer Consideration	Consultation and drop-in events have been undertaken in the preliminary stage for overall wider scheme
Carbon reduction	Improved pedestrian and cycling facilities will encourage more sustainable transport use
Wards	Croft, Wensleydale

Background Papers:

File reference – CE221004

Appendix A –

- CE221004-01-B1328-10-01_Rev- - Overall Scheme Layout
- CE221004-01-B1328-10-41_Rev- - Road Markings - Bridge St West
- CE221004-01-B1328-10-42_Rev- - Road Markings - Bridge St East
- CE221004-01-B1328-10-43_Rev- - Road Markings - Waterloo Rd East
- CE221004-01-B1328-10-44_Rev- - Traffic Signs - Bridge St West
- CE221004-01-B1328-10-45_Rev- - Traffic Signs - Bridge St East
- CE221004-01-B1328-10-46_Rev- - Traffic Signs - Waterloo Rd East
- CE221004-01-B1328-10-48_Rev- - Traffic Sign Schedule 1
- CE221004-01-B1328-10-49_Rev- - Traffic Sign Schedule 2
- CE221004-01-B1328-10-50_Rev- - Road Marking Schedule

Author

Report Author **William Binks, Programme Officer**

DECISION TAKEN

Title of Officer: Director of Environment & Transport

Subject: Proposed new traffic restrictions and traffic calming measures – The Link (Bridge Street), Blyth

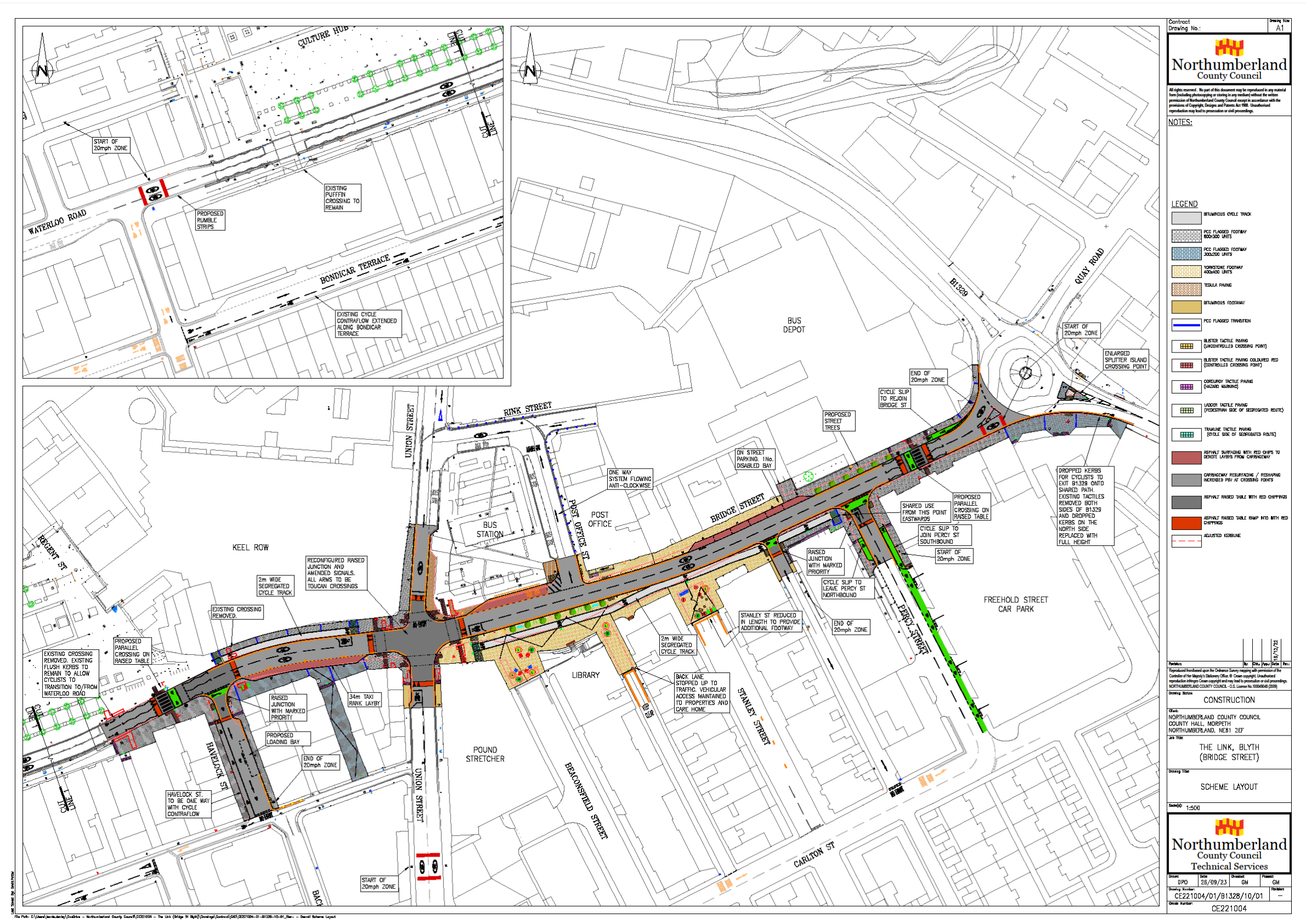
Consultation: Consultations with statutory consultees, affected businesses and members of the public undertaken by the Energising Blyth project team

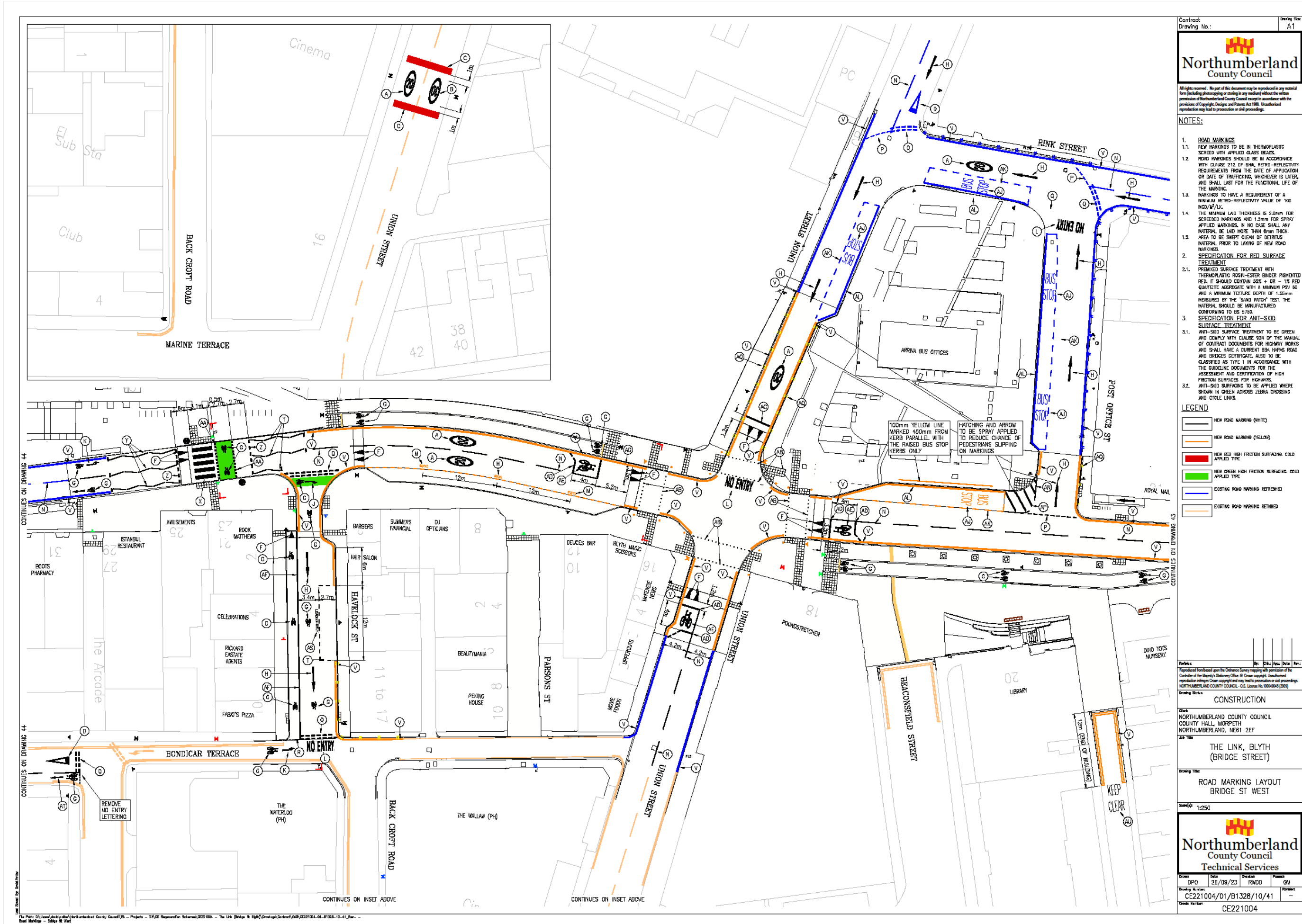
Decision Taken: To introduce new traffic restrictions and traffic calming measures in Bridge Street and its adjoining areas, Blyth

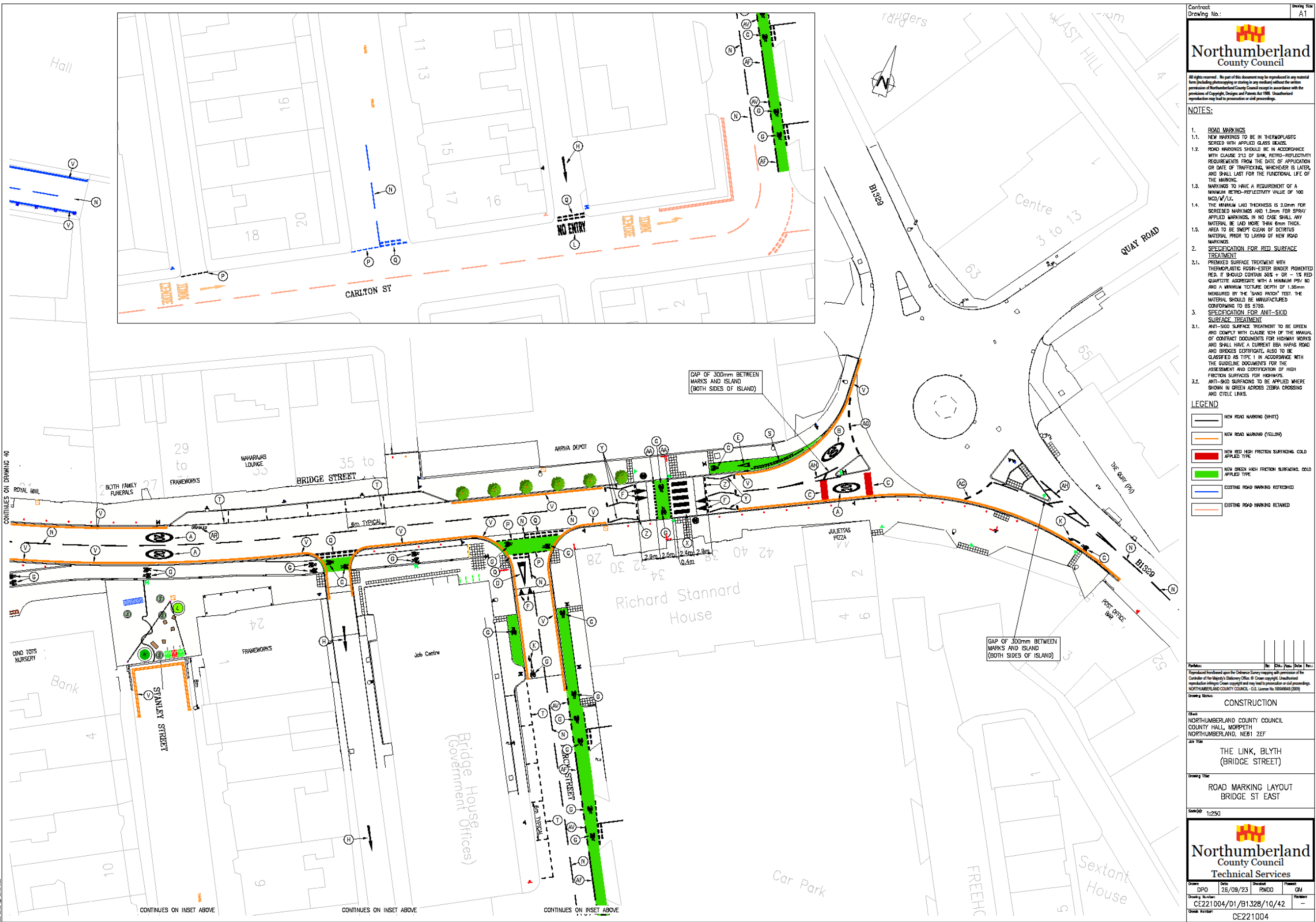
Signature of Director

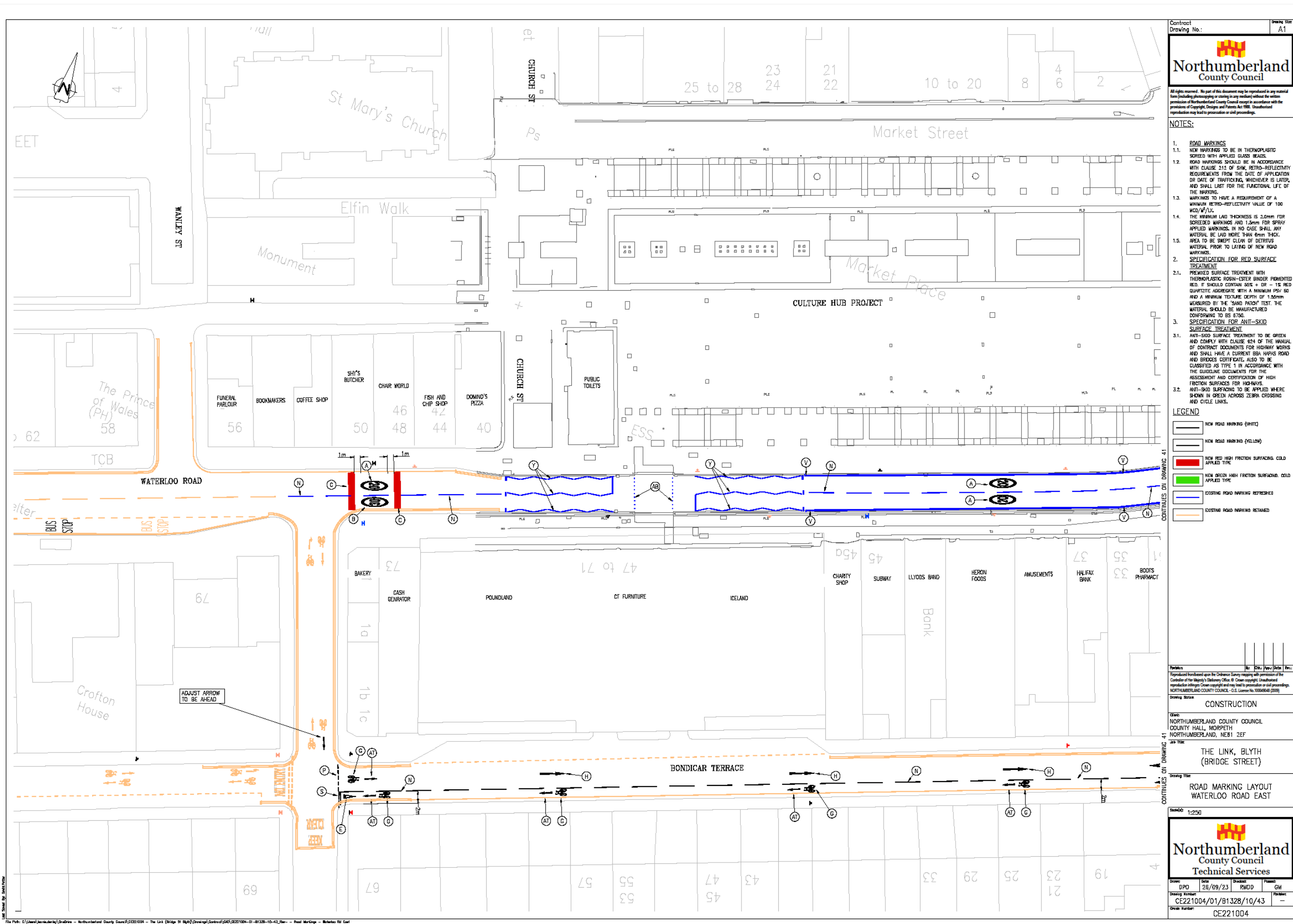
A handwritten signature in black ink, appearing to be 'JP Jnl', is written over a light blue horizontal line. The signature is cursive and stylized.

Date
21st December 2023









Contract No. 10004948
Drawing No. A1



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NOTES:

1. ROAD MARKINGS
- 1.1. NEW MARKINGS TO BE IN THERMOPLASTIC SCREENED WITH APPLIED GLASS BEADS.
- 1.2. ROAD MARKINGS SHOULD BE IN ACCORDANCE WITH CLAUSE 311.5 OF SMM, RETRO-REFLECTIVITY REQUIREMENTS FROM THE DATE OF APPLICATION OR DATE OF TRAFFIC LIGHTING, WHICHEVER IS LATER, AND SHALL LAST FOR THE FUNCTIONAL LIFE OF THE MARKING.
- 1.3. MARKINGS TO HAVE A REQUIREMENT OF A MINIMUM RETRO-REFLECTIVITY VALUE OF 100 MCD/MTX.
- 1.4. THE MINIMUM LINE THICKNESS IS 3.0mm FOR SCREENED MARKINGS AND 1.5mm FOR SPRAY APPLIED MARKINGS. IN NO CASE SHALL ANY MATERIAL BE LAYED MORE THAN 6mm THICK. AREA TO BE SWEEPED CLEAN OF DEBRIS MATERIAL PRIOR TO LAYING OF NEW ROAD MARKINGS.
2. SPECIFICATION FOR RED SURFACE TREATMENT
- 2.1. PRE-MIXED SURFACE TREATMENT WITH THERMOPLASTIC RESIN-ESTER UNDER PIGMENTED RED. IT SHOULD CONTAIN 50% OF 15 RED QUARTZITE AGGREGATE WITH A MINIMUM PSV 50 AND A MINIMUM TEXTURE DEPTH OF 1.5mm MEASURED BY THE "SAND PATCH" TEST. THE MATERIAL SHOULD BE MANUFACTURED CONFORMING TO BS 6750.
3. SPECIFICATION FOR ANTI-SKID SURFACE TREATMENT
- 3.1. ANTI-SKID SURFACE TREATMENT TO BE GREEN AND COMPLY WITH CLAUSE 425 OF THE MANUAL OF CONTRACT DOCUMENTS FOR HIGHWAY WORKS AND SHALL HAVE A CURRENT BBA HAPUS ROAD AND BRIDGES CERTIFICATE ALSO TO BE CLASSIFIED AS TYPE 1 IN ACCORDANCE WITH THE GUIDELINE DOCUMENTS FOR THE ASSESSMENT AND CERTIFICATION OF HIGH FRICTION SURFACES FOR HIGHWAYS.
- 3.2. ANTI-SKID SURFACING TO BE APPLIED WHERE SHOWN IN GREEN ACROSS ZEBRA CROSSING AND CYCLE LINES.

LEGEND	
	NEW ROAD MARKING (WHITE)
	NEW ROAD MARKING (YELLOW)
	NEW RED HIGH FRICTION SURFACING. COLOURED APPLIED TYPE
	NEW GREEN HIGH FRICTION SURFACING. COLOURED APPLIED TYPE
	EXISTING ROAD MARKING REFRESHED
	EXISTING ROAD MARKING RETAINED

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COUNTY HALL, MORPETH
NORTHUMBERLAND, NE61 2EF

Job Title:
THE LINK, BLYTH
(BRIDGE STREET)

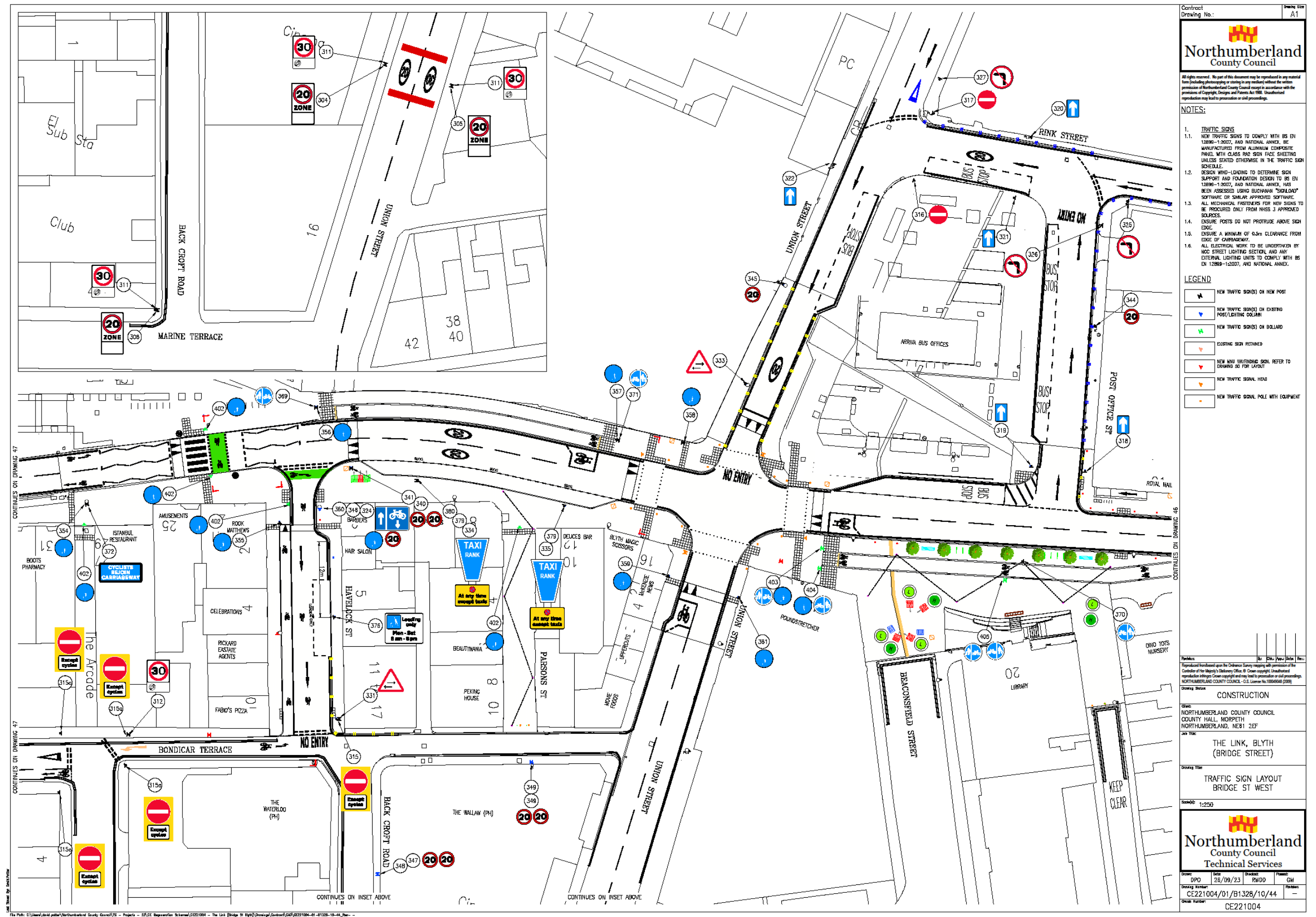
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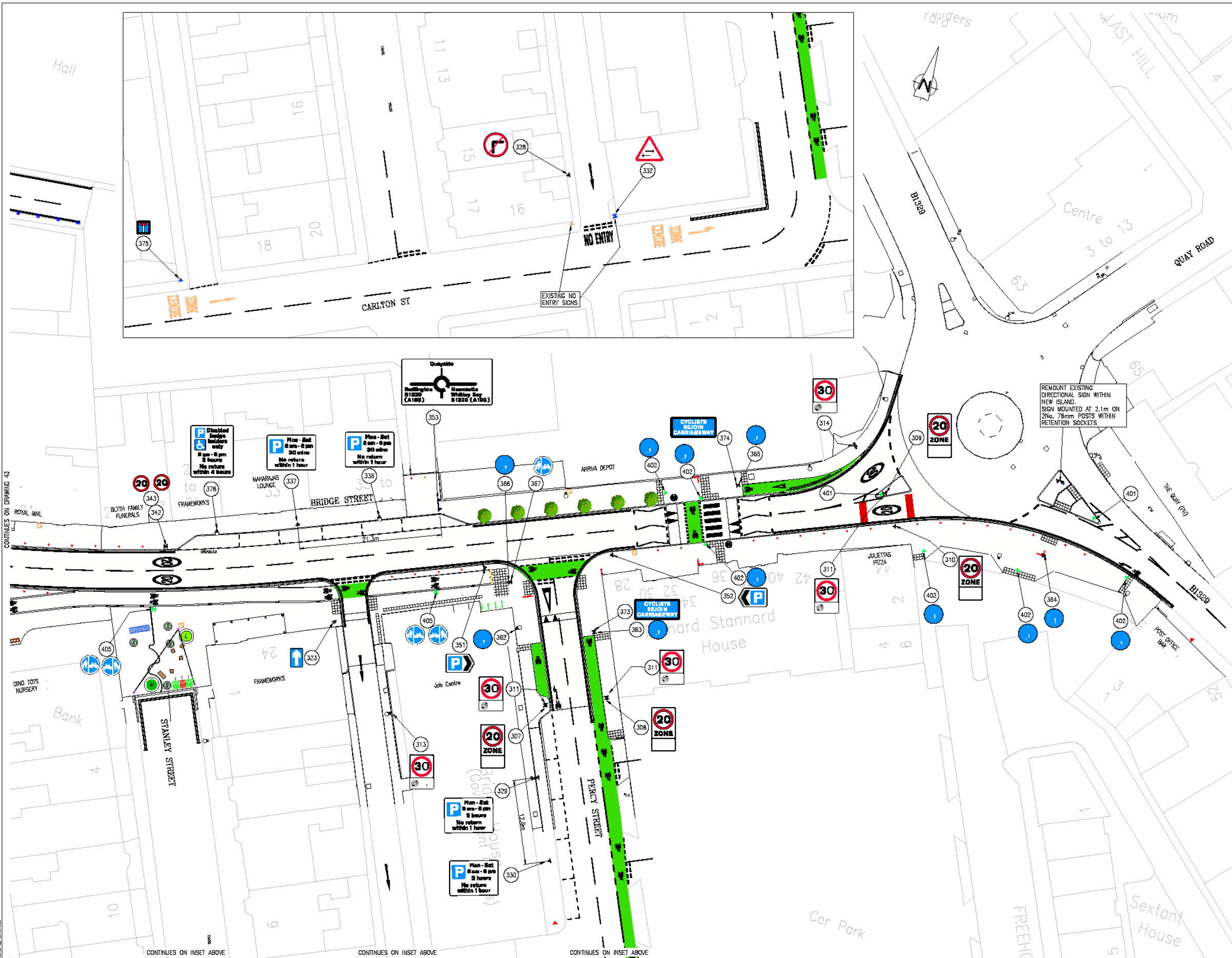
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CE221004





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NOTES:

1. TRAFFIC SIGNS

1.1. NEW TRAFFIC SIGNS TO COMPLY WITH BS EN 12899-1:2007, AND NATIONAL ANNEX, BE MANUFACTURED FROM ALUMINIUM COMPOSITE PANEL WITH CLASS RA2 SIGN FACE SHEETING UNLESS STATED OTHERWISE IN THE TRAFFIC SIGN SCHEDULE.

1.2. DESIGN AND LAYOUT TO DETERMINE SIGN SUPPORT AND FOUNDATION DESIGN TO BS EN 12899-1:2007, AND NATIONAL ANNEX, HAS BEEN ASSESSED USING EUCALAND "SIGNLOAD" SOFTWARE OR SIMILAR APPROVED SOFTWARE.

1.3. ALL MECHANICAL FASTENERS FOR NEW SIGNS TO BE PROVIDED ONLY FROM HISS 3 APPROVED SOURCES.

1.4. ENSURE POSTS DO NOT PROTRUDE ABOVE SIGN EDGE.

1.5. ENSURE A MINIMUM OF 0.5m CLEARANCE FROM EDGE OF CARPARKWAY.

1.6. ALL ELECTRICAL WORK TO BE UNDERTAKEN BY MCC STREET LIGHTING SECTION AND ANY EXTERNAL LIGHTING UNITS TO COMPLY WITH BS EN 12899-1:2007, AND NATIONAL ANNEX.

LEGEND

NEW TRAFFIC SIGN(S) ON NEW POST

NEW TRAFFIC SIGN(S) ON EXISTING POST/LIGHTING COLUMN

NEW TRAFFIC SIGN(S) ON ISLAND

EXISTING SIGN RETAINED

NEW WAY WITHDRAWING SIGN REFER TO DRAWING 50 FOR LAYOUT

NEW TRAFFIC SIGNAL HEAD

NEW TRAFFIC SIGNAL POLE WITH EQUIPMENT

Perkins:

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Client:

NORTHUMBERLAND COUNTY COUNCIL
COUNTY HALL, MORPETH
NORTHUMBERLAND, NE61 2EF

Job Title:

THE LINK, BLYTH
(BRIDGE STREET)

Drawing Title:

TRAFFIC SIGN LAYOUT
BRIDGE ST EAST

Scale:

1:250

Northumberland
County Council

Technical Services

DPO:

28/09/23

Drawn:

RMDD

Checked:

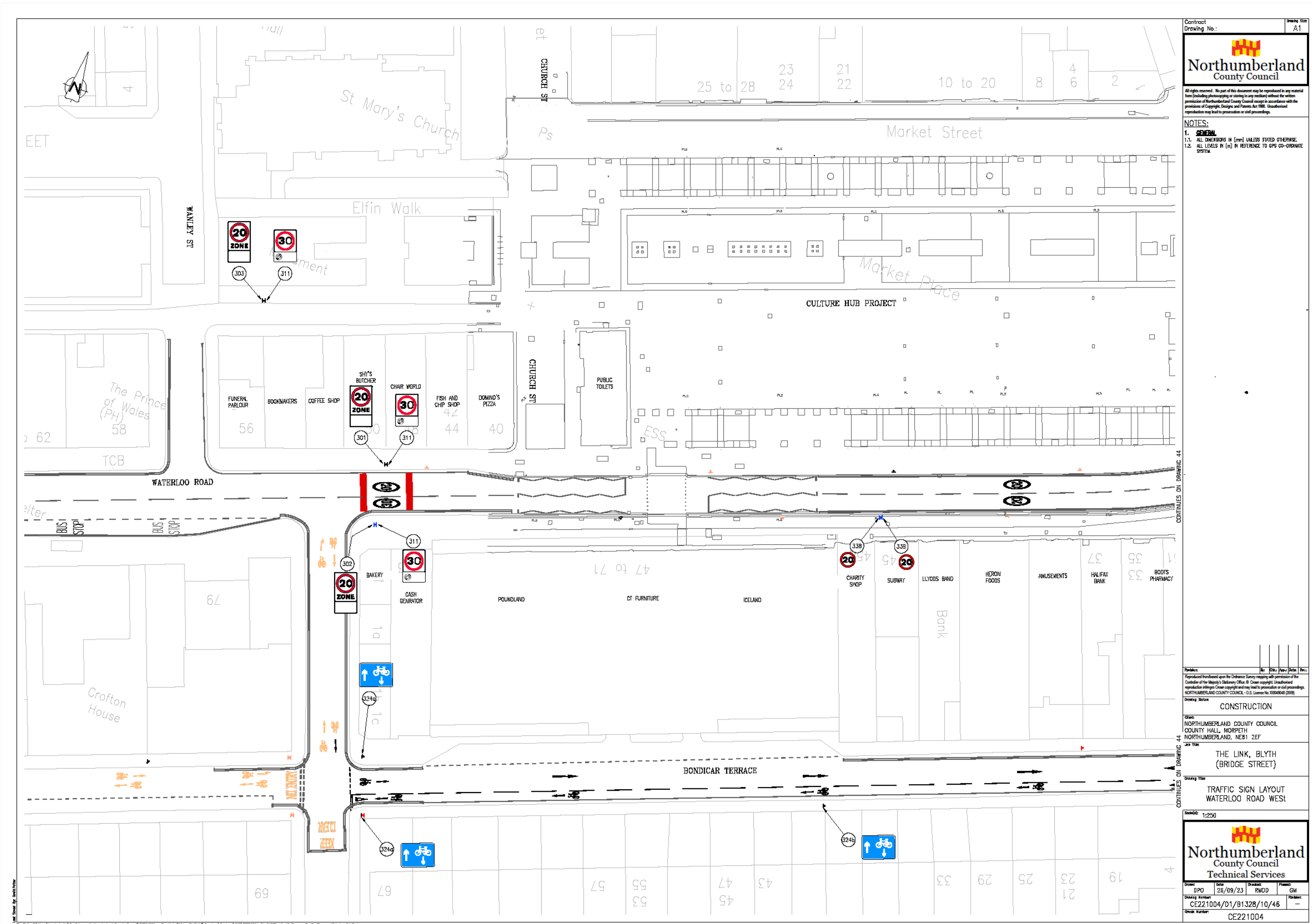
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CE221004/01/B1328/10/45




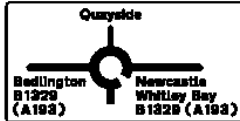








Order Number:

CE221004



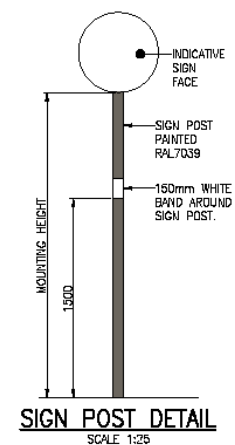
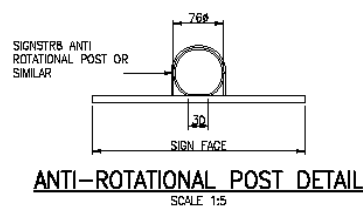
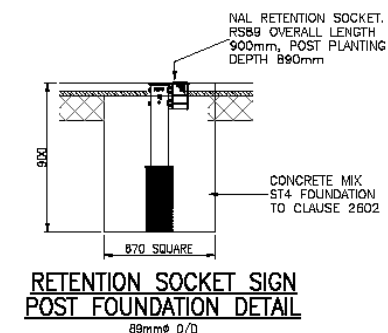
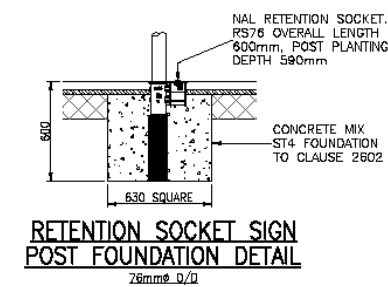
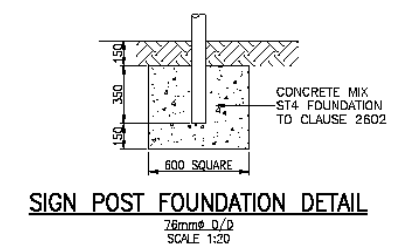
Delegated Decision

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322							-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN																																																																																																																																																																	
323							-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN																																																																																																																																																																	
324							76 signpost	RAL 7038	RS76 (P10590)	3.565	1	2.3	SIGN MOUNTED ON NEW 76mm POST INSTALLED IN LINE WITH CORNER OF FRAMEWORKS BUILDING																																																																																																																																																																	
324	680-1	-		1	CLASS R&Z	0.595 x 0.475 (0.31m x 0.31m)	-	-	-	-	-	2.6	SIGN MOUNTED ON RELOCATED COLUMN 680/78. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN																																																																																																																																																																	
324a							76mm WIDE BASED	0.595 x 0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)	0.475 (0.31m x 0.31m)

NEW POST MOUNTED SIGNS (300 SERIES)													
REF.	TSR&D 2015 D&G NO.	"X" HEIGHT (mm)	LEGEND	No. OFF	SURFACE	APPROX. SIZE (m)	POSTS / SIGNS				MOUNT HEIGHT (m)	REMARKS	
							DIA. (mm)	POST COLOUR	RETENTION SOCKET	LENGTH (m)	No.		
338	870	-		13	CLASS R&Z	0.3	-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN USING TERPA BAND. SIGN TO BE MOUNTED BELOW EXISTING PARKING RESTRICTION SIGN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
339							-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN USING TERPA BAND. SIGN TO BE MOUNTED BACK TO BACK WITH SIGN REF 338. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
340							-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 338
341							-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 338
342							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW POST TO SUP BOLLARD SPINDERS AS SHOWN ON PLAN
343							-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 342
344							-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
345							-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
346							-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
347							-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
348	-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN						
349	-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN						
350	-	-	-	-	-	2.3	SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN						
351	-	60		1	CLASS R&Z	0.940 x 0.450 (0.27m2)	76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST IN LINE WITH THE WEST EDGE OF THE TACTILE PAINTING. EQUIVALENT BETWEEN PM 1219 AND CARPENTRY KERS.
352	-	60		1	CLASS R&Z	0.840 x 0.430 (0.27m2)	-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 350
353	-	82.5		1	CLASS R&Z	2.035 x 1.635 (2.11m2)	88	RAL 7038	RS88 (P10680)	4.2	2	2.3	SIGN MOUNTED ON 316.08mm POSTS. NORTHERN POST TO BE IN LINE WITH THE EDGE OF THE DASHED AREA. SIGN TO BE COLOURED SO AS TO HIGHLIGHT THE AVAILABLE FOOTWAY WIDTH.
354	596	-		12	CLASS R&Z	0.3	-	-	-	-	-	2.3	SIGN MOUNTED ON NEW LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
356							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST LOCATED IN LINE WITH THE CARPENTRY KERS AS SHOWN ON PLAN
357							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST IN LINE WITH THE CYCLE WAY DEMARCATION BLOCKS
358							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST IN LINE WITH THE CYCLE WAY DEMARCATION BLOCKS
359							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST AT THE LOCATION SHOWN ON PLAN
360							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST NORTH OF THE HAZARD WARNING TACTILE AS CLOSE AS POSSIBLE TO THE NEWSAGENT'S WALL
361							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW LIGHTING COLUMN BACK TO BACK WITH REPEATER SIGN. RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
362							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
363							76 signpost	RAL 7038	RS76 (P10590)	3.355	1	2.3	SIGN MOUNTED ON NEW 76mm POST IN LINE WITH THE KERBLINE OF THE CYCLE SLIP FROM THE SHARED FOOTWAY
364							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST IN LINE WITH THE CENTRE OF THE TACTILE CROSSING POINT 2m GAP BETWEEN THE REAR OF THE TACTILES AND THE POST. WARNING SIGN MOUNTED ABOVE THIS SIGN.
365	76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST IN LINE WITH THE KERBLINE OF THE CYCLE SLIP FROM THE SHARED FOOTWAY						
366	76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST IN LINE WITH THE CYCLE WAY DEMARCATION BLOCKS						
367	857	-		3	CLASS R&Z	0.3	-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 368
368							76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST (VAL. POINT AGAINST CYCLOWAY BLOCK) AT THE TOP OF THE CYCLOWAY BLOCK TRANSITION
369							-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 368
370	857	-		2	CLASS R&Z	0.3	-	-	-	-	-	2.3	SIGN MOUNTED ON PESTON LIGHTING POLE. RUBBER ISOLATION BAND REQUIRED
371	-	-	-	-	-	-	-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 367
372	808	40		3	CLASS R&Z	0.66 x 0.51 (0.21m2)	-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 354 ON NEW LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
373							-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 363
374							-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN REF 365
375	818	-		1	CLASS R&Z	0.58 x 0.56	76 signpost	RAL 7038	NO	3.45	1	2.3	SIGN MOUNTED ON NEW 76mm POST AT A SIMILAR LOCATION TO REMOVED ONE WAY SIGN
376	-	-		1	CLASS R&Z	0.303 x 0.540 (0.07m2)	76 signpost	RAL 7038	RS76 (P10590)	3.310	1	2.3	SIGN MOUNTED ON NEW 76mm POST AT LOCATION SHOWN ON PLAN
377	-	-	-	-	-	-	not used				-	-	
378	-	25		2	CLASS R&Z	0.360 x 0.550 (0.06m2)	-	-	-	-	-	2.3	SIGN MOUNTED ON NEW LIGHTING COLUMN AS SHOWN ON PLAN. SIGN TO HAVE RUBBER ISOLATION BAND BETWEEN SIGN AND COLUMN
379	-	76 & 45 CAPS LETTER		2	WHITE CAL	0.35 TOP 0.15 BOTTOM 0.45 HEIGHT	-	-	-	-	-	3.0	4 SIDED INVERTED PYRAMID SHAPED SIGN FABRICATED BY MCC SIGN SHOP. SIGN MOUNTED AT THE TOP OF POSTS CARRIAGE SIGN REF 334 AND 338
380	TWO INFORMATION BOARD												

NEW BOLLARD MOUNTED SIGNS (400 SERIES)										
REF.	TSR&GD 2016 B&D NO.	T ₁ HEIGHT (mm)	LEGEND	No. OFF	SURFACE	APPROX. SIZE (m)	POSTS / SIGNS		MOUNT HEIGHT (m)	REMARKS
							ØA (mm)	LENGTH (m)		
401	805	—		2	CLASS RA2	0.3				SIGNMOUNTS WEDGEL BOLLARD MOUNTED ON TRAFFIC ISLAND FACING TRAFFIC APPROACHING ROUNDABOUT
402	906	—		11	CLASS RA2	0.15 (ROUND)	HERITAGE BOLLARD COLOURED RAL 7038		800 (UNDERSIDE OF ROUND)	SIGN FACE FIXED TO BOLLARD USING COUNTERSUNK SCREWS TO ATTACH SIGN PLATE. REAR OF SIGN PLATE TO BE COLOURED TO MATCH BOLLARD COLOUR. SIGN FACE REFLECTIVE VINYL ADHERED TO FACE OF PLATE ONCE FIXED TO BOLLARD BOLLARD INSTALLED AT LOCATION SHOWN ON PLAN INSTALLATION OF BOLLARD TO USE SAME DETAIL AS HERITAGE BOLLARD
403	909 & 907	—		1	CLASS RA2	0.15 (ROUND)	HERITAGE BOLLARD COLOURED RAL 7038		800 (UNDERSIDE OF ROUND)	SIGN FACE FIXED TO BOLLARD USING COUNTERSUNK SCREWS TO ATTACH SIGN PLATE. REAR OF SIGN PLATE TO BE COLOURED TO MATCH BOLLARD COLOUR. SIGN FACE REFLECTIVE VINYL ADHERED TO FACE OF PLATE ONCE FIXED TO BOLLARD BOLLARD INSTALLED AT LOCATION SHOWN ON PLAN INSTALLATION OF BOLLARD TO USE SAME DETAIL AS HERITAGE BOLLARD
404	909 & 907	—		1	CLASS RA2	0.15 (ROUND)	HERITAGE BOLLARD COLOURED RAL 7038		800 (UNDERSIDE OF ROUND)	SIGN FACE FIXED TO BOLLARD USING COUNTERSUNK SCREWS TO ATTACH SIGN PLATE. REAR OF SIGN PLATE TO BE COLOURED TO MATCH BOLLARD COLOUR. SIGN FACE REFLECTIVE VINYL ADHERED TO FACE OF PLATE ONCE FIXED TO BOLLARD BOLLARD INSTALLED AT LOCATION SHOWN ON PLAN INSTALLATION OF BOLLARD TO USE SAME DETAIL AS HERITAGE BOLLARD
405	907 & 907	—		3	CLASS RA2	0.15 (ROUND)	HERITAGE BOLLARD COLOURED RAL 7038		800 (UNDERSIDE OF ROUND)	SIGN FACE FIXED TO BOLLARD USING COUNTERSUNK SCREWS TO ATTACH SIGN PLATE. REAR OF SIGN PLATE TO BE COLOURED TO MATCH BOLLARD COLOUR. SIGN FACE REFLECTIVE VINYL ADHERED TO FACE OF PLATE ONCE FIXED TO BOLLARD BOLLARD INSTALLED AT LOCATION SHOWN ON PLAN INSTALLATION OF BOLLARD TO USE SAME DETAIL AS HERITAGE BOLLARD

NMU WAYFINDING SIGNS (500 SERIES)													
REF.	TSM&D 2018 DIM. NO.	H* HEIGHT (mm)	LEGEND	No. OFF	SURFACE	APPROX. SIZE (m)	POSTS / SIGNS			LENGTH (m)	No.	MOUNT HEIGHT (m)	REMARKS
							DIA. (mm)	POST COLOUR	RETENTION BUCKET				
501	-	30		1	CLASS R&Z	0.853 ± 0.306 (0.25m±2)	78 POST EXTENSION	G&V	NO	0.3	1	2.3	SIGN MOUNTED IN PLACE OF REMOVED SIGN IDENTIFIED ON ST1 CLIMBING CRACKED 300mm POST EXTENSION REQUIRED (OUT TO ADHESIVE MOUNTING HEIGHT)
502	-	30		2	CLASS R&Z	0.665 ± 0.160 (0.12m±2)	-	-	-	-	-	2.3	SIGN MOUNTED BACK TO BACK WITH SIGN 304A (CIRCLE CENTRAL) ON EXISTING POST
503	-	30		2	CLASS R&Z	0.665 ± 0.160 (0.12m±2)	78 SIGNETRS	G&V	NO	3.080	1	2.3	SINGLE SLOTTED END MOUNTED SIGN ON NEW POST AS CLOSE AS POSSIBLE TO SIGNPOST ON THE IMMEDIATE EAST SIDE OF THE ROAD INTO THE GATED TARD
504	-	30		1	CLASS R&Z	0.640 ± 0.166 (0.12m±2)	78 SIGNETRS	G&V	NO	3.060	1	2.3	SIGN MOUNTED ON NEW POST IN PLACE OF BOLLARD AT THE LOCATION SHOWN ON PLAN
505	-	30		1	CLASS R&Z	0.740 ± 0.170 (0.13m±2)	78 SIGNETRS	R&V 7030	R576 (PPO200)	3.060	1	2.3	DOUBLE SLOTTED END MOUNTED SIGN MOUNTED ON NEW POST 300mm EAST OF TACTILE PARKING AND 2m NORTH OF S&P FRONT OF THE LOCATION SHOWN ON PLAN
506	-	30		1	CLASS R&Z	0.640 ± 0.200 (0.14m±2)	-	-	-	-	-	SEE REMARKS	DOUBLE SLOTTED END MOUNTED SIGN SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. ENSURE SIGN IS VISIBLE USING HOODING SIGN ALONG BOMBARDIER TERRACE AND NOT DISTURBED BY PLUS SHOP FRONT MIN MOUNT HEIGHT 2.3
507	-	30		1	CLASS R&Z	0.640 ± 0.166 (0.12m±2)	78 SIGNETRS	R&V 7030	R576 (PPO200)	3.055	1	2.3	DOUBLE SLOTTED END MOUNTED SIGN SIGN MOUNTED ON NEW POST IN LINE WITH THE NORTHERN EXTENT OF THE TACTILE PARKING AND 300mm EAST OF THE TACTILE AS SHOWN ON PLAN
508	-	30		3	CLASS R&Z	0.645 ± 0.168 (0.11m±2)	78 SIGNETRS	R&V 7030	R576 (PPO200)	3.055	1	2.3	DOUBLE SLOTTED END MOUNTED SIGN SIGN MOUNTED ON NEW POST 300mm EAST OF THE TACTILE ST LINE WITH THE CORNER OF THE FOOTWAY AS SHOWN ON PLAN
509	-	30		3	CLASS R&Z	0.645 ± 0.168 (0.11m±2)	78 SIGNETRS	R&V 7030	R576 (PPO200)	3.055	1	2.3	SINGLE SLOTTED END MOUNTED SIGN ON NEW POLE 1.2m WEST OF THE EDGE OF CARBAMANT AND 300mm SOUTH OF THE TACTILES
510	-	30		1	CLASS R&Z	0.682 ± 0.163 (0.09m±2)	-	-	-	-	-	2.6	DOUBLE SLOTTED END MOUNTED SIGN SIGN MOUNTED ABOVE SHARED UP SIGN REF 304 DIRECTING VEHICLE DOWN SPLITTER ISLAND
511	-	30		1	CLASS R&Z	0.640 ± 0.170 (0.11m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN MOUNTED ON NEW POST 0.6m FROM THE EDGE OF THE TACTILE AS SHOWN ON PLAN. SIGN ON SAME POST AS SIGN REF 512
512	-	30		1	CLASS R&Z	0.665 ± 0.170 (0.12m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN MOUNTED ON SAME POST AS SIGN REF 511
513	-	30		1	CLASS R&Z	0.640 ± 0.168 (0.11m±2)	78 SIGNETRS	R&V 7030	R576 (PPO200)	3.055	1	2.3	SINGLE SLOTTED END MOUNTED SIGN SIGN MOUNTED 300mm WEST OF TACTILE STEM AS SHOWN ON PLAN POST TO BE INSTALLED AS CLOSE AS POSSIBLE TO SHOP FRONT
514	-	30		3	CLASS R&Z	0.740 ± 0.170 (0.13m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN MOUNTED ON SAME POST AS SIGN REF 508
515	-	30		3	CLASS R&Z	0.740 ± 0.170 (0.13m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN MOUNTED ON SAME POST AS SIGN REF 508
516	-	30		1	CLASS R&Z	0.565 ± 0.178 (0.11m±2)	-	-	-	-	-	2.6	DOUBLE SLOTTED END MOUNTED SIGN MOUNTED ON SAME POST AS SIGN REF 510 AND 304A
517	-	30		1	CLASS R&Z	0.638 ± 0.160 (0.11m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN MOUNTED ON SAME POST AS SIGN REF 506
518	-	30		2	CLASS R&Z	0.655 ± 0.168 (0.09m±2)	78 SIGNETRS	R&V 7030	R576 (PPO200)	3.055	1	2.3	DOUBLE SLOTTED END MOUNTED SIGN MOUNTED ON NEW POST 300mm WEST FROM THE TACTILES AND AS CLOSE AS POSSIBLE TO THE CORNER OF THE FOOTWAY
519	-	30		2	CLASS R&Z	0.655 ± 0.168 (0.09m±2)	78 SIGNETRS	R&V 7030	R576 (PPO200)	3.055	1	2.3	DOUBLE SLOTTED END MOUNTED SIGN MOUNTED ON NEW POST IN LINE WITH THE CORNER OF THE BUILDING AS SHOWN ON PLAN
520	-	30		1	CLASS R&Z	0.640 ± 0.170 (0.11m±2)	-	-	-	-	-	2.3	DOUBLE SLOTTED END MOUNTED SIGN MOUNTED ON SAME POST AS SIGN REF 513
521	-	30		1	CLASS R&Z	0.665 ± 0.170 (0.12m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN MOUNTED ON SAME POST AS SIGN REF 505
522	-	30		1	CLASS R&Z	0.638 ± 0.160 (0.11m±2)	78 SIGNETRS	R&V 7030	R576 (PPO200)	3.070	1	2.3	DOUBLE SLOTTED END MOUNTED SIGN MOUNTED ON NEW POST IN LINE WITH THE CORNER OF THE BUILDING AS SHOWN ON PLAN
523	-	30		2	CLASS R&Z	0.780 ± 0.330 (0.28m±2)	78 SIGNETRS	G&V	NO	3.225	1	2.3	SIGN MOUNTED ON NEW POST ON THE EAST SIDE OF THE PARKING LINE WITH THE SHARED POST AT THE NAGLES AS SHOWN ON PLAN
524	-	30		1	CLASS R&Z	0.565 ± 0.200 (0.14m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN MOUNTED ON EXISTING COLUMN AS SHOWN ON PLAN
525	-	30		1	CLASS R&Z	0.560 ± 0.200 (0.14m±2)	78 SIGNETRS	G&V	NO	3.150	1	2.3	SINGLE SLOTTED END MOUNTED SIGN ON NEW POST AS CLOSE AS POSSIBLE TO THE SHARED CHANNEL AND LOCATED CENTRAL TO THE BRICK PAVEL ON THE BUILDING
526	-	30		1	CLASS R&Z	0.665 ± 0.170 (0.12m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN SIGN MOUNTED ON EXISTING LIGHTING COLUMN AS SHOWN ON PLAN. ENSURE SIGN IS VISIBLE USING HOODING SIGN ALONG HAYLOCK STREET. SIGN MOUNTED AT SAME HEIGHT AS SIGN REF 50
527	-	30		1	CLASS R&Z	0.350 ± 0.135 (0.07m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN MOUNTED ON SAME POST AS SIGN REF 518
528	-	30		1	CLASS R&Z	0.640 ± 0.170 (0.11m±2)	-	-	-	-	-	2.3	SINGLE SLOTTED END MOUNTED SIGN MOUNTED ON SAME POST AS SIGN REF 518
529	-	30		1	CLASS R&Z	0.655 ± 0.170 (0.12m±2)	-	-	-	-	-	2.3	DOUBLE SLOTTED END MOUNTED SIGN SIGN MOUNTED ON SAME POST AS SIGN REF 507



Contract Drawing No.:	Drawing Size: A1
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Northumberland
County Council

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NOTES:

1. **TRAFFIC SIGNS**
- 1.1. NEW TRAFFIC SIGNS TO COMPLY WITH BS EN 12869-1:2007, AND NATIONAL ANNEX, BE MANUFACTURED FROM ALUMINUM COMPOSITE PANEL WITH CLASS R42 SKIN FACE SHEETING UNLESS STATED OTHERWISE IN THE TRAFFIC SIGN SCHEDULE.
- 1.2. DESIGN WIND-LOADING TO DETERMINE SIGN SUPPORT AND FOUNDATION DESIGN TO BS EN 12869-1:2007, AND NATIONAL ANNEX, HAS BEEN ASSESSED USING BUCHHEIM "SIGNLOAD" SOFTWARE OR SIMILAR APPROVED SOFTWARE.
- 1.3. ALL MECHANICAL FASTENERS FOR NEW SIGNS TO BE PROVIDED ONLY FROM NHSS 3 APPROVED SOURCES.
- 1.4. ENSURE POSTS DO NOT PROTRUDE ABOVE SIGN EDGE.
- 1.5. ENSURE A MINIMUM OF 0.5m CLEARANCE FROM EDGE OF CARPARKWAY.

Drawing: **Electrical** CONSTRUCTION

Client: NORTHUMBERLAND COUNTY COUNCIL
COUNTY HALL, MORPETH
NORTHUMBERLAND, NE61 3EE

THE LINK, BLYTH
(BRIDGE STREET)

TRAFFIC SIGN SCHEDULE
SHEET 2

Size (in)	AS SHOWN
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Northumberland
County Council
Technical Services

Drawn: DPO	Date: 26/09/23	Drawn by: RWDD	Passed: GM
Drawing Number: CE221004/01/B1328/10/49			Revised: —
Grade Number: CE221004			

ROAD MARKINGS SCHEDULE				
REF	DIAG. No.	DIAGRAM	MARK (mm)	USE
A	1055		4300	SMALL 20 ROUND
B	1055		4300	SMALL 30 ROUND
C	-		TRANSVERSE	1m BAND OF RED HATCHED SURFACING
D	1023A		3750	GIVE WAY TRIANGLE
E	1023B		1675	GIVE WAY TRIANGLE (CYCLES)
F	1056		EDGE TO TOP OF HUMP	EDGE OF HUMP
G	1057		1215	SMALL CYCLE SYMBOL
H	1026		4000	AHEAD ARROW
J	1059		2000	RIGHT ARROW (CYCLES)
K	1059		2000	LEFT ARROW (CYCLES)
L	1046		1800	NO ENTRY LETTERING
M	1028.2		350	TAXI LETTERING
U	1004		4000	ADVISORY CYCLE LANE MARKING LINE
P	1006A		600	GIVE WAY EXTENSION
Q	1003A		600	GIVE WAY LINES
R	1006B		300	GIVE WAY EXTENSION (CYCLES)
S	1003B		300	GIVE WAY LINES (CYCLES)
T	1028.4		600	PARKING SPACES
U	1028.2		600	TAXI RANK EDGE
V	1010.1		CONTINUOUS	DOUBLE YELLOW LINE PARKING RESTRICTION
W	1017		CONTINUOUS	SINGLE YELLOW LINE PARKING RESTRICTION

ROAD MARKINGS SCHEDULE				
REF	DIAG. No.	DIAGRAM	MARK (mm)	USE
1	1021.5		800	ZEBCRA MARKS
1	1021.4		2000	CONTROLLED AREA STOP LINES
2	1021.5		500	GIVE WAY MARKINGS FOR PARALLEL CROSSING
AL	1055.3		250	ROUTE FOR PEDAL CYCLES AT CROSSING
AD	1026.1		100	PEDESTRIAN CROSSING STUDS
AD	1021		CONTINUOUS	STOP LINE
AD	1001.5A		CONTINUOUS	ADVANCE STOP LINE
AD	1001.2A		2750	ADVANCE STOP LINE CYCLE
AD	1049B		CONTINUOUS	EDGE OF CYCLE TRACK LINE
AD	1003.1		1000	ROUNDABOUT GIVE WAY
AD	1040		VARIES	AREA HATCHING BOUNDARY
AD	1023.1		1800	BUS STOP LETTERING
AD	1023.1		1000	BUS STOP BOUNDARY
AD	1023.1		CONTINUOUS	BUS STOP KERB LINE
AD	1008.1		990	SLOW LETTERING (CYCLES)
AD	1040.4		VARIES	AREA HATCHING BOUNDARY
AD	1020.1		250	KERB BULGE
AD	1028.4		6.35	DISABLED BAY LETTERING
AD	1028.4		6.35	LOADING ONLY LETTERING

ROAD MARKINGS SCHEDULE				
REF	DIAG. No.	DIAGRAM	MARK (mm)	USE
AD	1008		3000	AHEAD ARROW (CYCLES)
AD	1026		1800	KEEP CLEAR LETTERING
AD			1000	MANDATORY CYCLING AT JUNCTIONS

TO BE READ IN CONJUNCTION WITH TSP-002 2018

Contract Drawing No.: A1

Northumberland County Council

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NOTES:

- ROAD MARKINGS
 - NEW MARKINGS TO BE IN THERMOPLASTIC SCALED WITH APPLIED GLASS BEADS.
 - ROAD MARKINGS SHOULD BE IN ACCORDANCE WITH CLAUSE 21.1 OF SHW. RETRO-REFLECTIVITY REQUIREMENTS FROM THE DATE OF APPLICATION OR DATE OF TRAFFICKING, WHICHEVER IS LATER, AND SHALL LAST FOR THE FUNCTIONAL LIFE OF THE MARKING.
 - MARKINGS TO HAVE A REQUIREMENT OF A MINIMUM RETRO-REFLECTIVITY VALUE OF 100 MCD/M²/LX.
 - THE MINIMUM LAY THICKNESS IS 2.0mm FOR SCALED MARKINGS AND 1.5mm FOR SPRAY APPLIED MARKINGS. IN NO CASE SHALL ANY MATERIAL BE LAY MORE THAN 6mm THICK.
 - AREA TO BE SWEEP CLEAN OF DEBRIS MATERIAL PRIOR TO LAYING OF NEW ROAD MARKINGS.
- SPECIFICATION FOR RED SURFACE TREATMENT
 - PREMIXED SURFACE TREATMENT WITH THERMOPLASTIC RESIN-ESTER BINDER PIGMENTED RED. IT SHOULD CONTAIN 50% ± 0% OF 15 RED QUARTZITE AGGREGATE WITH A MINIMUM PSV 80 AND A MINIMUM TEXTURE DEPTH OF 1.50mm MEASURED BY THE "SAND PATCH" TEST. THE MATERIAL SHOULD BE MANUFACTURED CONFORMING TO BS 6750.
- SPECIFICATION FOR ANTI-SKID SURFACE TREATMENT
 - ANTI-SKID SURFACE TREATMENT TO BE GREEN AND COMPLY WITH CLAUSE 8.4 OF THE MANUAL OF CONTRACT DOCUMENTS FOR HIGHWAY WORKS AND SHALL HAVE A CURRENT BBA HAPAS ROAD AND BRIDGES CERTIFICATE. ALSO TO BE CLASSIFIED AS TYPE 1 IN ACCORDANCE WITH THE GUIDELINE DOCUMENTS FOR THE ASSESSMENT AND CERTIFICATION OF HIGH FRICTION SURFACES FOR HIGHWAYS.
 - ANTI-SKID SURFACING TO BE APPLIED WHERE SHOWN IN GREEN ACROSS ZEBRA CROSSING AND CYCLE LINES.

Author	Checked	Approved	Drawn

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Drawing Status: CONSTRUCTION

Client: NORTHUMBERLAND COUNTY COUNCIL COUNTY HALL, MORPETH NORTHUMBERLAND, NE61 2EF

Job Title: THE LINK, BLYTH (BRIDGE STREET)

Drawing Title: ROAD MARKING SCHEDULE

Scale: AS SHOWN

Northumberland County Council

Technical Services

Drawn	Date	Checked	Project
DPO	25/09/23	RWD	GH
Drawing Number:	CE221004/01/B1328/10/50	Project:	-
Drawn Number:	CE221004	Project:	-

File Path: C:\Users\jessie\Documents - Northumberland County Council\CE221004 - The Link (Bridge St Blyth)\Drawings\Control\GH\CE221004-01-B1328-10-50_Road Marking Schedule